

Spending on Transportation Projects in

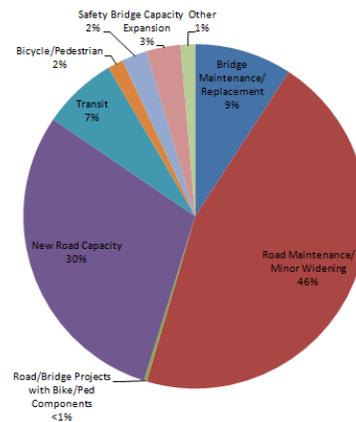
Wisconsin



Wisconsin's 2011-2014 Statewide Transportation Improvement Program uses ranges to estimate the anticipated funding for all non-transit projects and some transit projects.* An examination of the STIP reveals that road maintenance/minor widening projects make up the largest chunk of the planned spending (46 percent). New road capacity projects, which include the North-South Freeway project and the Depere-Suamico project, follow at 30 percent of the planned spending. Bridge maintenance/replacement projects make up 9 percent, and transit projects make up 7 percent.** Bridge capacity expansion projects account for 3 percent of the planned expenditures, and safety and bicycle/pedestrian projects 2 percent each. Projects classified as "other" make up 1 percent of the spending, and road or bridge projects with bicycle/pedestrian components (such as adding sidewalks when reconstructing a roadway) comprise less than 1 percent.

Proposed Spending, FY 2011-2014

Project Type	Cost (\$ millions)
Bridge Maintenance/Replacement	501.18
Road Maintenance/Minor Widening	2,480.68
Road/Bridge Projects with Bike/Ped Components	15.75
New Road Capacity	1,621.18
Transit **	386.54
Bicycle/Pedestrian	80.03
Safety	129.50
Bridge Capacity Expansion	175.98
Other	74.85
Total**	\$5,465.66



* In order to complete the analysis, TSTC assigned costs to the state's estimations by taking the average of the range provided; a project estimated as "\$60-69.99 million" is recorded as \$65 million.

** Transit component includes \$239.51 million in anticipated federal assistance to rural transit providers. State and local contributions to federal transit assistance programs are not listed in the STIP nor is transit assistance to the Milwaukee or Madison transit systems. However, with data provided by WisDOT, this analysis includes an estimation of federal assistance for 2011-2014 totaling \$103.08 million for the Milwaukee and Madison transit systems. WisDOT provided TSTC with the 2011 federal dollar amount and TSTC multiplied by 4 to approximate the amount for the entire 4 year period. Transit total includes \$12.68 million in ARRA funds for the Amtrak Hiawatha commuter service, but does not include \$900,000 in federal and state funds for the Milwaukee-Minneapolis/St. Paul high-speed rail environmental impact statement.

These summary sheets were produced by the Tri-State Transportation Campaign, a non-profit organization working toward more balanced, transit-friendly and equitable transportation policies in Connecticut, New York and New Jersey. The full report is available at Trackstatedollars.org. The analysis was conducted using the Statewide Transportation Improvement Plan (STIP). TSTC staff analyzed each individual project listing in the STIP, categorizing projects by either an assigned project type where available, or by the project description and supporting documentation. This analysis examines all projects listed in the STIP. States are required to list all transportation projects that receive federal aid, though many states also include projects that only receive state dollars. However, there may be certain projects not included in the STIP, such as those undertaken by toll road authorities or through state infrastructure banks. All percentages have been rounded. For more information on the methodology utilized, please contact TSTC. Image of Wisconsin from 50states.com. This project was made possible by support from the Rockefeller Foundation.

TRI-STATE TRANSPORTATION CAMPAIGN

