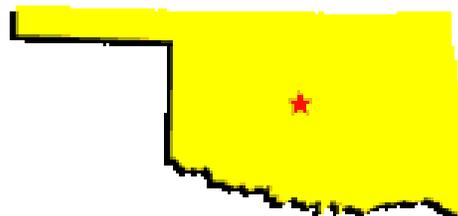


Spending on Transportation Projects in

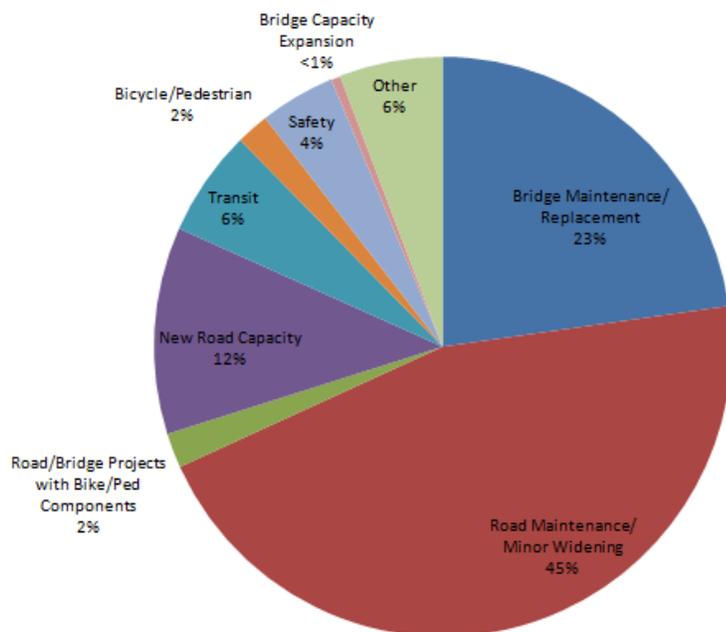
Oklahoma



An examination of Oklahoma's 2011-2014 Statewide Transportation Improvement Program reveals that road maintenance/minor widening projects account for the largest chunk of the proposed expenditures (45 percent). Bridge maintenance/replacement projects follow, at 23 percent of the proposed spending. New road capacity projects comprise 12 percent. Transit projects and projects classified as "other" each make up 6 percent of the STIP. Safety projects* account for 4 percent of the planned spending. Bicycle/pedestrian projects make up 2 percent of the STIP, as do road or bridge projects with bicycle/pedestrian components (such as adding sidewalks when reconstructing a roadway). Bridge capacity expansion projects make up less than 1 percent of the planned spending.

Proposed Spending, FY 2011-2014

Project Type	Cost (\$millions)
Bridge Maintenance/Replacement	1,371.21
Road Maintenance/Minor Widening	2,738.37
Road/Bridge Projects with Bike/Ped Components	116.92
New Road Capacity	698.32
Transit	358.46
Bicycle/Pedestrian	110.56
Safety*	254.44
Bridge Capacity Expansion	31.15
Other	348.19
Total	\$6,027.62



* Total safety spending includes \$10.17 million on projects with bicycle or pedestrian components.

These summary sheets were produced by the Tri-State Transportation Campaign, a non-profit organization working toward more balanced, transit-friendly and equitable transportation policies in Connecticut, New York and New Jersey. The full report is available at Trackstatedollars.org. The analysis was conducted using the Statewide Transportation Improvement Plan (STIP). TSTC staff analyzed each individual project listing in the STIP, categorizing projects by either an assigned project type where available, or by the project description and supporting documentation. States are required to list all transportation projects that receive federal aid in the STIP, though many states also include projects that only receive state dollars as well. However, there may be certain projects not included in the STIP, such as those undertaken by toll road authorities of state infrastructure banks. All percentages have been rounded. For more information on the methodology utilized, please contact TSTC. Image of Oklahoma from 50states.com. This project was made possible by support from the Rockefeller Foundation.

TRI-STATE TRANSPORTATION CAMPAIGN

