

Spending on Transportation Projects in

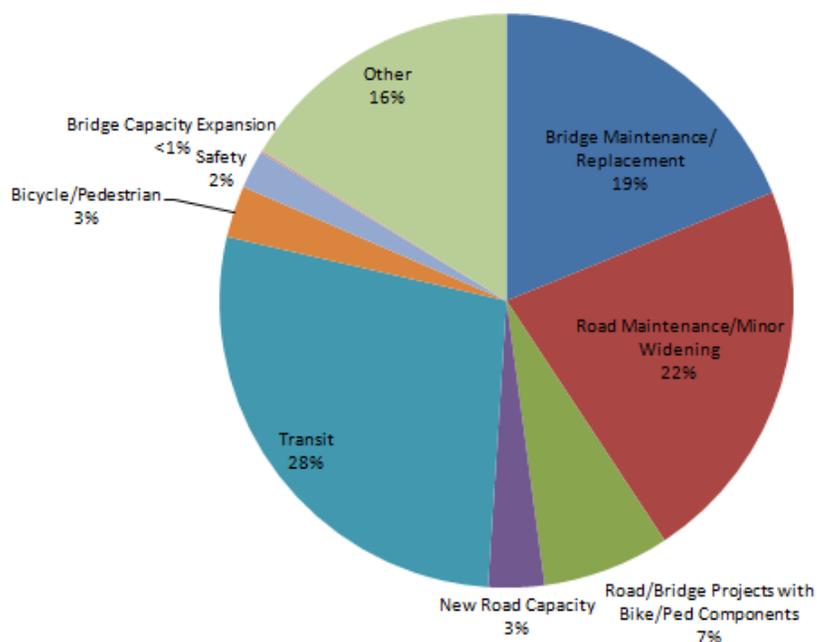
Maine

An examination of Maine's 2010-2013 Statewide Transportation Improvements Program reveals that transit projects account for the largest chunk of planned spending (28 percent).^{*} Road maintenance/minor widening and bridge maintenance/replacement follow, 22 percent and 19 percent respectively. The "other" category accounts for 16 percent of the planned spending and includes items such as general planning expenditures, freight costs and information technology. Road or bridge projects with bicycle/pedestrian components (such as adding sidewalks when reconstructing a roadway) make up 7 percent of planned spending. Bicycle/pedestrian projects comprise 3 percent of the program, as do new road capacity projects. Safety projects comprise 2 percent of the program, and, at less than 1 percent, bridge capacity expansion projects make up the smallest share of the proposed spending.



Proposed Spending, FY 2010-2013

Project Type	Cost (\$millions)
Bridge Maintenance/Replacement	181.10
Road Maintenance/Minor Widening	210.41
Road/Bridge Projects with Bike/Ped Components	68.88
New Road Capacity	30.31
Transit*	264.91
Bicycle/Pedestrian	28.06
Safety	20.64
Bridge Capacity Expansion	1.20
Other	156
Total	\$961.51



* The transit component includes a \$110 million in ARRA funds for High-Speed Intercity Passenger Rail.

These summary sheets were produced by the Tri-State Transportation Campaign, a non-profit organization working toward more balanced, transit-friendly and equitable transportation policies in Connecticut, New York and New Jersey. The full report is available at Trackstatedollars.org. The analysis was conducted using the Statewide Transportation Improvement Plan (STIP). TSTC staff analyzed each individual project listing in the STIP, categorizing projects by either an assigned project type where available, or by the project description and supporting documentation. States are required to list all transportation projects that receive federal aid in the STIP, though many states also include projects that only receive state dollars as well. However, there may be certain projects not included in the STIP, such as those undertaken by toll road authorities of state infrastructure banks. All percentages have been rounded. For more information on the methodology utilized, please contact TSTC. Image of Maine from 50states.com. This project was made possible by support from the Rockefeller Foundation.

